



Internal Air Transport Certification

ASC/ENFC (ATTLA)
2145 Monahan Way
WPAFB, OH 45433-7017
<https://afkm.wpafb.af.mil/AirTransport>



Date: 6 July 2011

Item Nomenclature: 2011 Ford F-350 Crew Cab 4x4

File Number: 2010.11.11 Rev 1

Requestor: 137th ALFC/NGB (SMSgt John Wesley)

Superseded Certification Date: 9 December 2010

New Information Summary: Permanent approval issued after identification of main frame locations for restraint.

Item Description: The subject item is identified to be a two axle 4x4 truck. Its approximate dimensions are 269" L x 96" W x 80" H. The vehicle weighs 6,750 lbs and has a gross vehicle weight rating (GVWR) of 10,800 lbs. The front axle is reported to weigh 4,000 lbs and a rating of 4,800 lbs. The rear axle is reported to weigh 2,750 lbs and a rating of 6,290 lbs. This certification applies to any Ford F-350 with same dimensions and ratings regardless of the year of manufacture.



Figure 1: 2011 Ford F-350 Crew Cab

Certified Aircraft: USAF C-130, C-17, and C-5

Conditions of Approval:

1. Maximum Weight for Air Transport:

- a. Gross Vehicle Weight: 8,640 lbs (80% rated capacity)
- b. Axle Limits:
 - 1) Front – 3,840 lbs (No shoring, 80% rated capacity); 4,800 (with Sleeper Shoring)
 - 2) Rear - 5,032 lbs (80% of rated capacity)

2. Item Preparation:

- a. Shipper is responsible for sleeper shoring, see paragraph 5 for dimensions.
- b. All hazardous materials (to include fuel level, batteries, etc.) must be prepared and certified for airlift in accordance with TM 38-250/AFMAN 24-204(I). Do not consider this air transport document

as approval for hazardous materials. Authorization for airlifting hazardous material is the responsibility of 401 SCMS/GUMAA (DSN 787-4503 or COM (937) 257-4503).

3. Loading Instructions:

- a. The vehicle can be loaded using general loading procedures as listed in the respective aircraft cargo loading manual.
- b. Sleeper shoring is required for the front axle if the weight exceeds 3,840 lbs. See paragraph 5 for minimum shoring dimensions.

4. Restraint Requirements: The 2011 Ford F-350 Crew Cab and all accompanying cargo must be restrained to meet MIL-HDBK-1791 requirements of 3G forward, 1.5G aft and lateral, and 2G up. In addition, stored or installed equipment must meet these requirements and be capable of withstanding a 4.5G down load. Restrain vehicle using largest frame components, preferably at longitudinal main frames adjacent to lateral cross members. Figures 2 and 3 illustrate identified frame restraint locations. Utilize 10K chains for the restraint of the vehicle. Restraint may be applied to axles up to their rated capacity listed under Item Description paragraph. Do not apply more than 50% of required restraint to vehicle axles for any given direction. Note that spring mounted axles provide zero vertical restraint.



Figure 2: Rear Frame Location



Figure 3: Forward Frame Location

5. Required Shoring Dimensions:

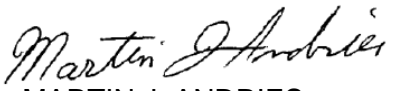
- a. Sleeper Shoring (if required): Minimum base dimensions shall be 7" L x 7" W (2 stacks). The sleeper shoring must be stacked to within 1/2" of chassis/frame area and secured with cargo straps.

Required Distribution:

1. Shipper shall give a copy of this approval to the ATOC representative when the item is presented for airlift. This memo shall be part of the official cargo manifest documentation package and shall be briefed to the aircraft loadmaster prior to loading this item.
2. AMC/A3V.

Point of Contact: MSgt Thomas McPeak, at thomas.mcpeak@wpafb.af.mil, DSN 986-9903 or Commercial (937) 656-9903. Refer to file number 2010.11.11 Rev 1 to reference this item.


Reviewed by: MELVIN C.J. SANTIAGO
Aerial Delivery Technical Expert
Crew Systems Branch


Approved by: MARTIN J. ANDRIES
Acting Technical Advisor
Crew Systems Branch