



Internal Air Transport Certification

AFLCMC/EZFC (ATTLA)
2145 Monahan Way
WPAFB, OH 45433-7017
<https://cs3.eis.af.mil/sites/AFLCMCEZF/AirCerts>



Date: 2 December 2013

Item Nomenclature: JISCC B3 Trailer

File Number: 2013.10.34

Requestor: US NAVY SCRB (Timothy D. Wenzler)

Item Description: The subject item is identified to be a JISCC B3 Trailer. The approximate reported dimensions are 239" L x 97" W x 104" H. The reported gross weight is 8,310 lbs with a gross vehicle weight rating (GVWR) of 10,000 lbs. Both axles are reported to weigh 3,725 lbs with a rating of 6,000 lbs. The landing gear/tongue has a reported weight of 860 lbs and has a rated capacity of 1,500 lbs.

NOTE: The listed dimensions are approximations. Actual dimensions can vary as long as aircraft limitations published in applicable cargo loading manuals and the conditions of certification listed below are not violated. Adjustments to approach shoring and/or loading methods shall be reported to ATTLA.



Figure 1: JISCC B3 Trailer

Certified Aircraft: USAF C-130, C-17, and C-5

Conditions of Certification:

1. Maximum Weight for Air Transport:

- a. Gross Vehicle Weight: 10,000 lbs (GVWR)
- b. Both Axles: 4,800 lbs each (without sleeper shoring); 6,000 lbs each (with sleeper shoring)
- c. Tongue/landing gear: 1,500 lbs (shoring limitation)

2. Item Preparation:

- a. Jack stands shall not be deployed.
- b. When using a prime mover, the trailer should be hitched such that the vertical distance between the bottom of the tongue frame and the ground is approximately 16" H. See Figure 2.
- c. Approach shoring should not be necessary if the distance between the rear axle of the prime mover and the hitch receiver is less than or equal to 53". A distance greater than 53" may require approach shoring with dimensions varying with aircraft and prime mover.
- d. Shipper is responsible for providing all shoring materials. See paragraphs 3 and 5 for specifications.
- e. To comply with rapid decompression requirements for air transport, the relief window or side door should be secured open.
- f. All equipment shall be prepared, packaged, or mounted such that there is no adverse effect on the functioning of the equipment after being subjected to the aircraft environmental extremes. MIL-STD-810 provides guidance on approved test methods and data gathering techniques.
- g. All hazardous materials (to include fuel level, batteries, etc.) must be prepared and certified for airlift in accordance with TM 38-250/AFMAN 24-204(I). Do not consider this air transport certification as approval for hazardous materials. Authorization for airlifting hazardous material is the responsibility of AFMC/A4RT (DSN 787-4503 or COM (937) 257-4503).

3. Loading Instructions:

- a. The Item can be loaded using general loading procedures as listed in the respective aircraft cargo loading manual.
- b. See paragraph 2b and 2c to see if approach shoring is needed.
- c. Sleeper shoring shall be placed under the frame near the axles on each side (2 stacks per axle) for each axle that exceeds paragraph 1b. See paragraph 5a for dimensions.
- d. Support shoring is required under the tongue if the trailer is disconnected from the prime mover. During flight the landing gear shall not be deployed. See paragraph 5b for dimensions.
- e. C-130: Trailer must be backed-on. A validation load has been conducted on a C-130. Photos of critical areas follow.



Figure 2: Distance from the ground to the bottom of the trailer frame. Should be 16" but was set to 14" for the validation load.



Figure 3: Ramp Contact condition is close but not touching



Figure 4: Tongue nearly touches ramp

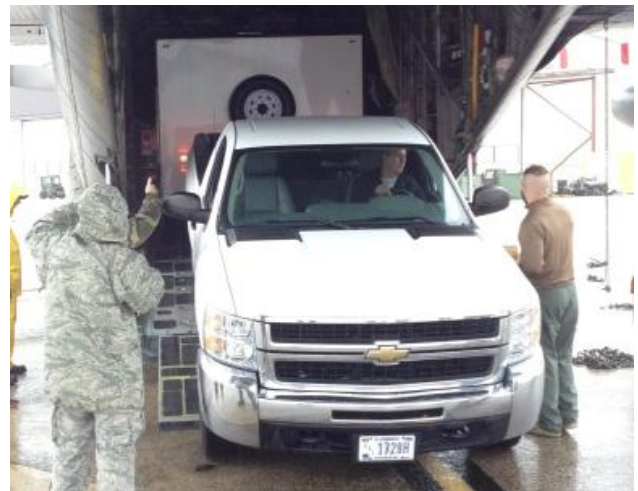


Figure 5: Trailer is close to aircraft walls

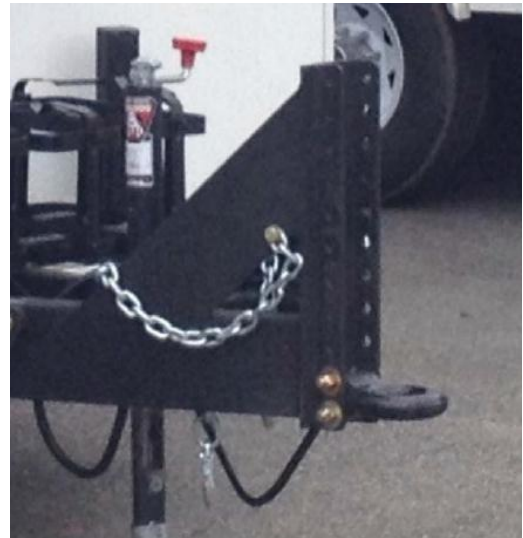


Figure 6: Trailer nearly touches C-130 Ceiling

4. Restraint Requirements: The JISCC B3 Trailer and all accompanying cargo must be restrained to meet MIL-STD-1791 requirements of 3G forward, 1.5G aft and lateral, and 2G up. In addition, stowed or installed equipment must meet these requirements and be capable of withstanding a 4.5G down load. The trailer is equipped with eight, 5K tiedown points (Figure 7). Additional restraint may be obtained by using the lunette ring (Figure 8), rated at 10K. Restraint may be applied to axles up to their axle ratings in longitudinal and lateral directions as listed in the Item Description paragraph. Do not apply more than 50% of required restraint to vehicle axles for longitudinal and lateral directions. Spring mounted axles provide zero vertical restraint.



**Figure 7: Tiedown Point
(5K Rated Capacity)**



**Figure 8: Lunette Ring
(10K Rated Capacity)**

5. Shoring Dimensions:

- a. Sleeper Shoring (if required per paragraph 1b): 8" L x 8" W, two stacks per axle. The sleeper shoring must be built up so that it is snug against the frame and is level for flight. Secure to the cargo floor.
- b. Support Shoring for Tongue (if required per paragraph 3d): 6" L x 6" W with height sufficient to maintain trailer level. Secure to the cargo floor.

Required Distribution:

1. Shipper shall give a copy of this certification to the ATOC representative when the item is presented for airlift. This memo shall be part of the official cargo manifest documentation package and shall be briefed to the aircraft loadmaster prior to loading this item.
2. AMC/A3V & AMC/A4T.
3. SDDC TEA.

Point of Contact: Caleb Wagner, at caleb.wagner@us.af.mil or ATTLA@us.af.mil, DSN 986-9955 or Commercial (937) 656-9955. Refer to file number 2013.10.34 to reference this item.

Approved by: MARK A. KUNTAVANISH
Aerial Delivery Technical Expert
Crew Systems Branch